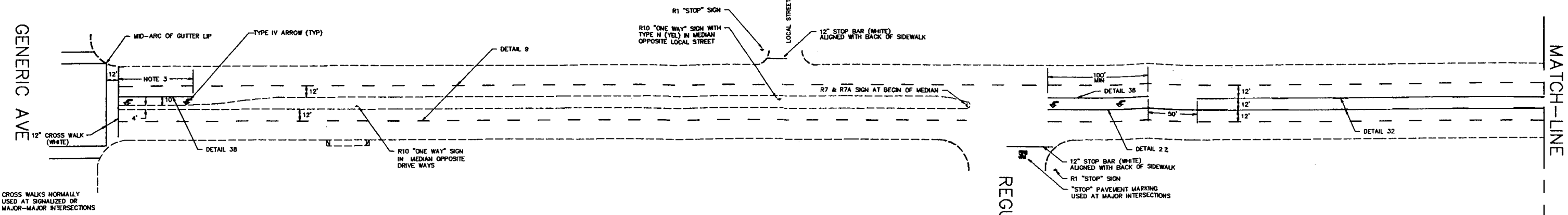


GENERIC AVE

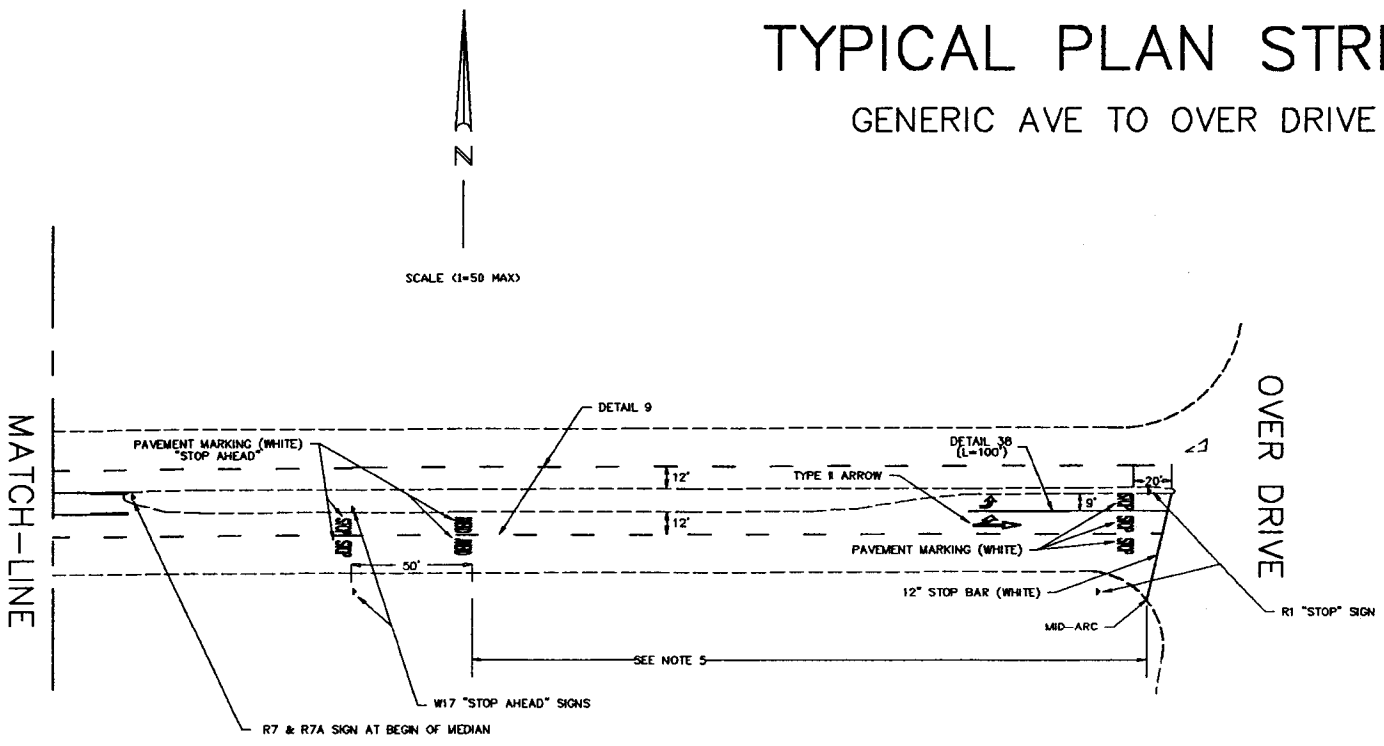
MATCH-LINE



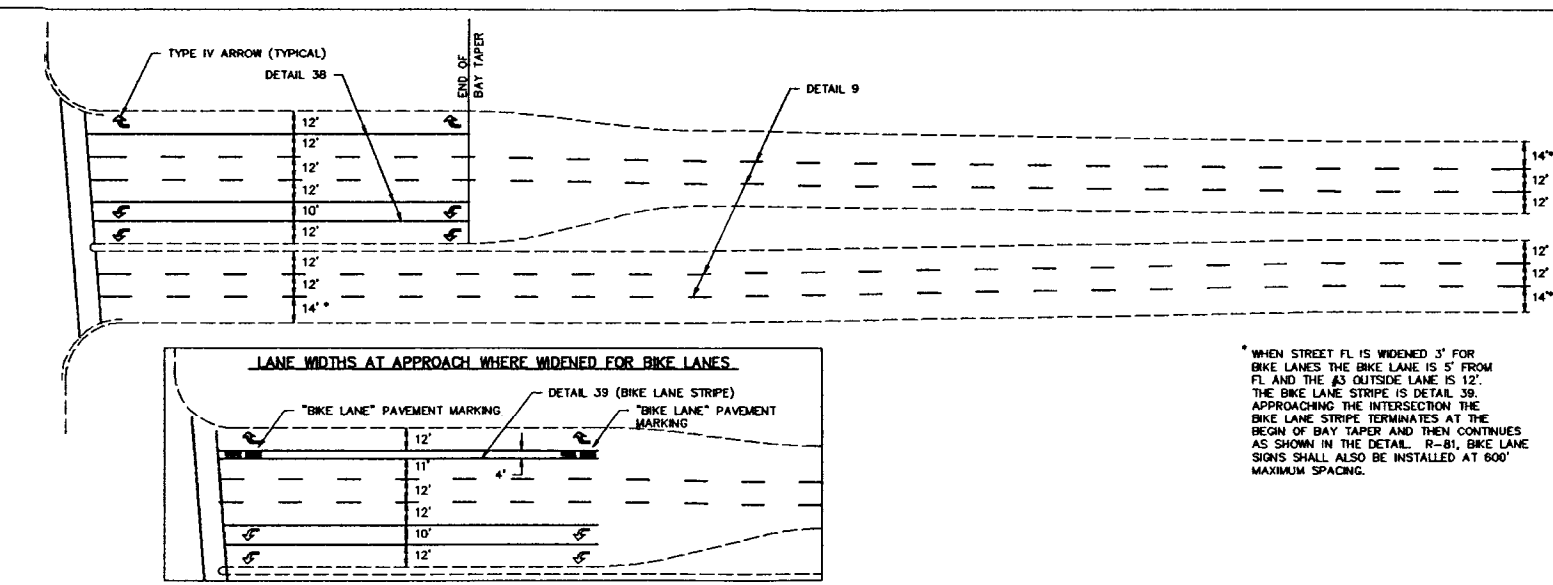
CROSS WALKS NORMALLY USED AT SIGNALIZED OR MAJOR-MAJOR INTERSECTIONS

## TYPICAL PLAN STREET

GENERIC AVE TO OVER DRIVE



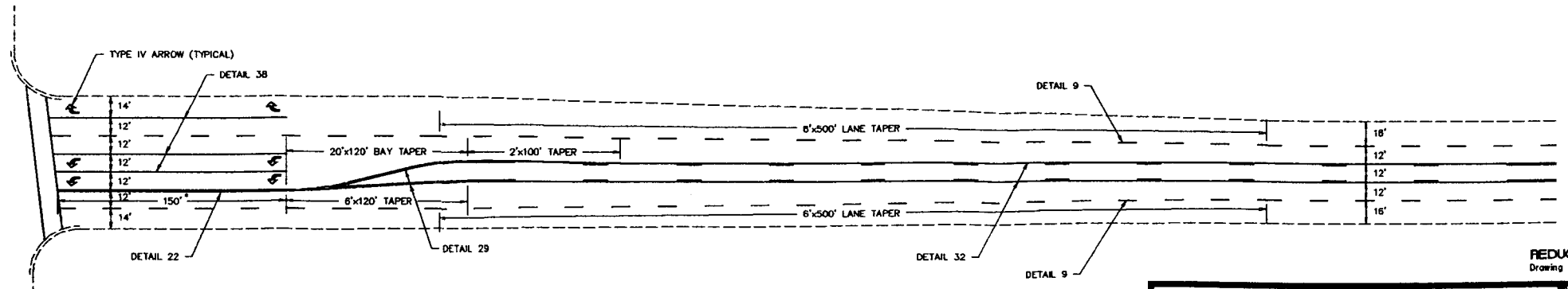
SCALE (1=50 MAX)



\* WHEN STREET FL IS WIDENED 3' FOR BIKE LANES THE BIKE LANE IS 5' FROM FL AND THE #3 OUTSIDE LANE IS 12'. THE BIKE LANE STRIPE IS DETAIL 39. APPROACHING THE INTERSECTION THE BIKE LANE STRIPE TERMINATES AT THE BEGIN OF BAY TAPER AND THEN CONTINUES AS SHOWN IN THE DETAIL. R-81, BIKE LANE SIGNS SHALL ALSO BE INSTALLED AT 600' MAXIMUM SPACING.

## TYPICAL EXPANDED ARTERIAL APPROACH

- GENERAL NOTES:
- FIGURES 6-13 THROUGH 6-55 SHALL BE USED AS A DESIGN GUIDE FOR STRIPING, SIGNING & MARKING PLANS.
  - STRIPING DETAILS AND PAVEMENT MARKINGS SHALL CONFORM TO CALTRANS STANDARD PLANS A20A THROUGH A24C.
  - STORAGE LENGTH FOR LEFT AND RIGHT TURN LANES VARY BASED ON NEED DUE TO DECELERATION AND ANTICIPATED VOLUMES. THE LENGTH SHALL MATCH LENGTH TO BEGINNING OF TURN BAY TAPERS WHERE EXISTING IN MEDIAN OR IN CURB AND GUTTER.
  - SIGNS SHALL BE PLACED 0.5' FROM BACK OF SIDEWALK WHERE COMBINATION TYPE SIDEWALK IS PRESENT OR DESIGNED. IF SIDEWALK IS EXISTING IT SHALL BE ROTO-HAMMERED AND SIGN INSTALLED. ALL SIGNS SHALL BE MOUNTED WITH HAWKINS M2G SERIES BOLT AND VANDAL PROOF NUT ASSEMBLY, OR EQUAL.
  - PLACEMENT OF WARNING SIGNS SHALL BE BASED ON SEC. 4-02.3 OF THE CALTRANS TRAFFIC MANUAL. IN THE ABSENCE OF SPECIFIC DATA IN THE TRAFFIC MANUAL TABLE II-1, "A GUIDE FOR ADVANCE WARNING SIGN PLACEMENT DISTANCE" FROM THE MUTCD (MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES) MAY BE USED.
  - TRANSITION TAPERS SHALL BE ADJUSTED IN THE FIELD TO PROVIDE SMOOTH TRANSITION OF TRAFFIC.



\* 150' STORAGE LANE IS MINIMUM FOR COLLECTOR EXPANDED COLLECTOR CAN PROVIDE UP TO 250' BACK TO BEG OF CURB TAPER. TRANSITION IN MEDIAN WOULD SHIFT ACCORDINGLY.

## TYPICAL EXPANDED COLLECTOR APPROACH

REDUCED 50%  
Drawing File: PCHKSTD

<p>TYPICAL STRIPING PLAN</p>		DATE	4/14/93
		DRAWN	B.D.
<p>CITY OF BAKERSFIELD CALIFORNIA</p>		DESIGNED BY	B.J. DEETER
		CHECKED BY	S. L. WALKER
<p>PUBLIC WORKS - TRAFFIC ENGINEERING</p>		<p>No 1 of 1</p>	<p>1</p>