



**PUBLIC WORKS DEPARTMENT**

1501 TRUXTUN AVENUE  
BAKERSFIELD, CALIFORNIA 93301  
(661) 326-3724

**RAUL M. ROJAS, DIRECTOR • CITY ENGINEER**

November 22, 2004

**Attn: Developers, Engineers, and Contractors**

**Re: Revised Curb Ramp Details**

CalTrans has issued a memorandum dated October 7, 2004 regarding revised curb ramp details RSP A88A, A88B, A90A, and A90B. Copies of A88A and A88B are attached. These can also be downloaded at the following web site:

[www.dot.ca.gov/hq/esc/oe/project\\_plans/HTM/02\\_plans\\_disclaim\\_dual.htm](http://www.dot.ca.gov/hq/esc/oe/project_plans/HTM/02_plans_disclaim_dual.htm)

Effective immediately, all new curb ramps within the City's jurisdiction shall comply with the revised state standards. The City will issue a revised curb ramp "Subdivision and Engineering Design Manual" standard when complete.

Both Federal and State laws require the revised standard for all public accessible ramps, not just for federally or state funded projects and highways. The most notable changes are the elimination of the ¼ inch lip at the bottom of the ramp (ramp to be flush with flowline), the replacement of a detectable warning surface for the herringbone pattern grooves, and the maximum flare slope reduction from 12.5% maximum to 10% maximum. Also note that the maximum slope of the gutter adjoining the ramp is 5%.

If you have any questions or need additional information, please feel free to call Stuart Patteson, Construction Superintendent, or Steve Lesh, Construction Supervisor, at 661-326-3049.

Sincerely,

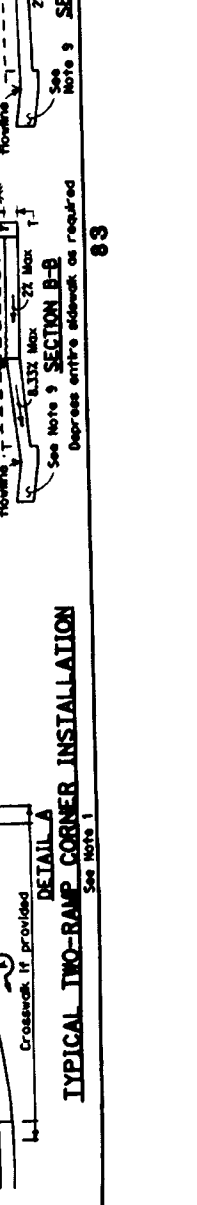
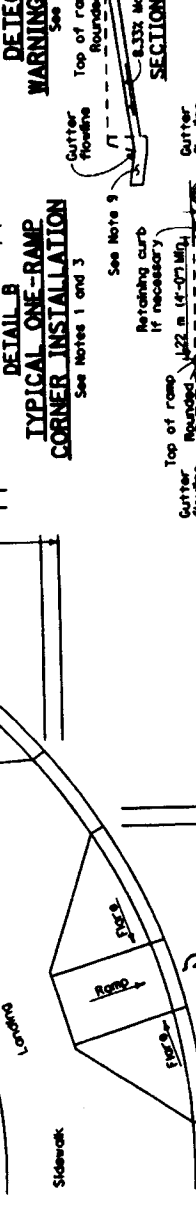
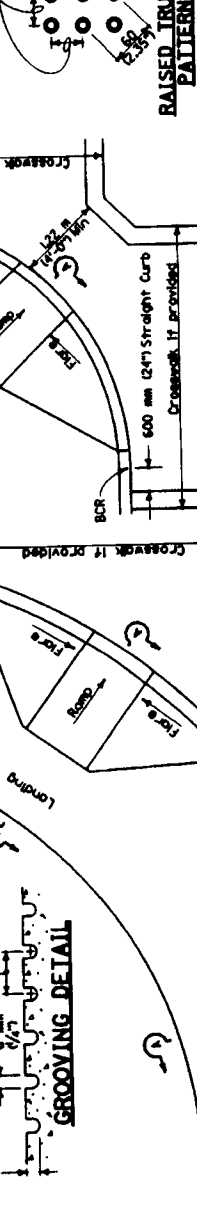
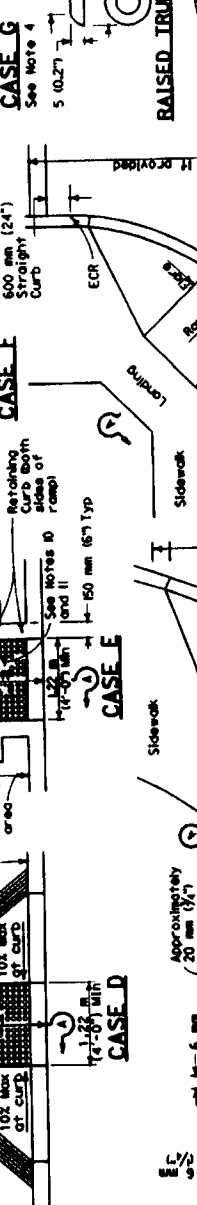
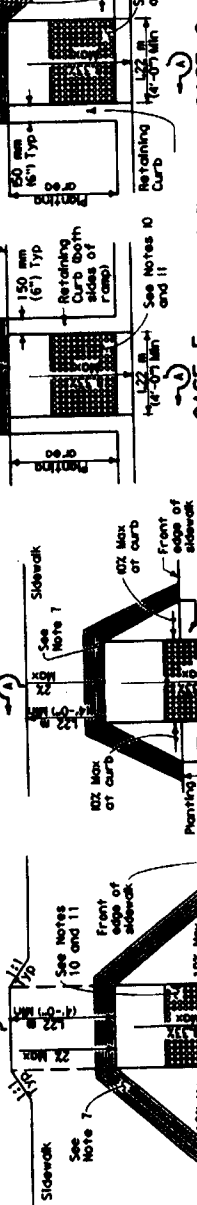
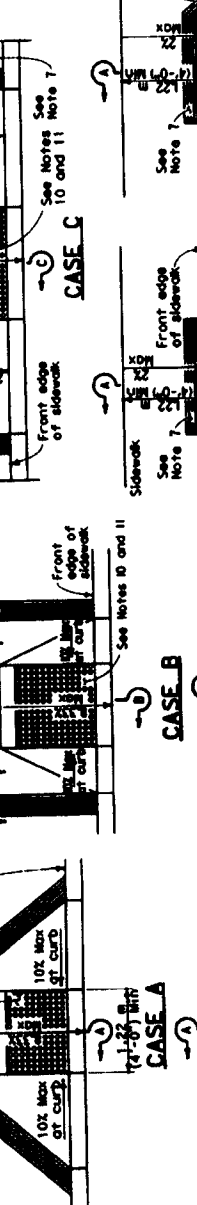
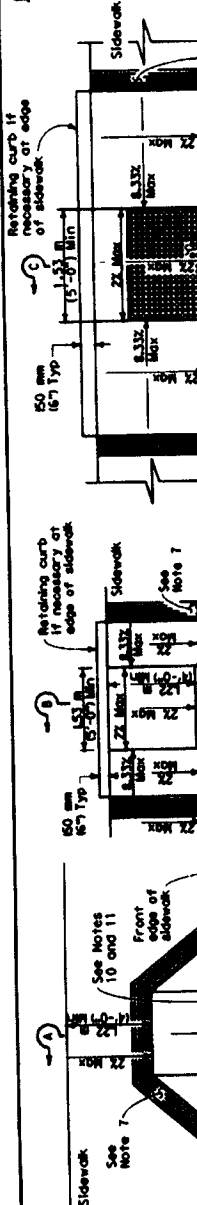


**Raul M. Rojas**  
Public Works Director

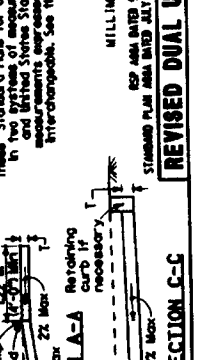
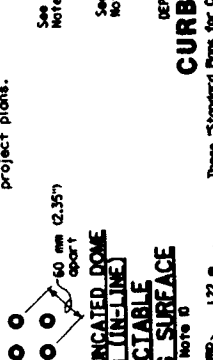
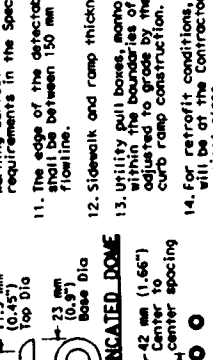
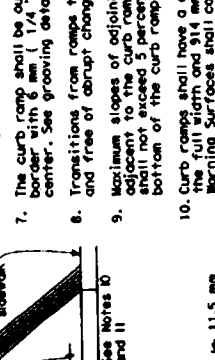
cc: Construction File  
Jacques LaRoche, Assistant Public Works Director  
Marian Shaw, Civil Engineer IV – Subdivisions  
Steve Walker, Traffic Engineer  
Stuart Patteson, Construction Superintendent  
Steven Lesh, Construction Supervisor  
Rick Milwee, Construction Supervisor

DIST. COUNTY	ROUTE	ALTERNATE POST TOTAL SHEETS	SHEET TOTALS
H. Paul Cook REGISTERED CIVIL ENGINEER		9-29-04	
PLANS APPROVAL DATE The State of California, Department of Transportation Standard Plans Unit, 1205 L Street, Sacramento, CA 95833			
California law (Section 17) requires that you get the plans and specifications from the Department of Transportation.			
To accompany plans dated:			

- NOTES**
- As site conditions dictate, Case A used through Case C curb ramps may be used for corner installations. Detail A and Detail B, the cases of curb ramps used in Detail A, do not have to be the same. Case A through Case C curb ramps also may be used at mid block locations, as site conditions dictate.
  - If distance from curb to back of sidewalk is too short to accommodate ramp and 1.22 m (4'-0") platform (landing) as shown in Case A, the sidewalk may be depressed longitudinally as in Case B, or C or may be sloped as in Case D, similar to that shown for Detail B.



- Maximum slopes of adjoining gutters, the road surface immediately adjacent to the curb ramp and continuous passage to the curb ramp shall not exceed 5 percent within 1.22 m (4'-0") of the top or bottom of the curb ramp.
- Curb ramps shall have a detectable warning surface that extends 1.22 m (4'-0") beyond the curb ramp. The detectable warning surface shall conform to the requirements in the Special Provisions.
- The edge of the detectable warning surface nearest the street shall be between 150 mm (6") and 205 mm (8") from the gutter line.
- Sidewalk and ramp thickness, "T", shall be 90 mm (3 1/2") minimum.
- Utility pull boxes, manholes, vaults and all other utility facilities within the boundaries of the curb ramp shall be adjusted to grade by the owner prior to, or in conjunction with, curb ramp construction.
- For retrofit conditions, removal and replacement of curb apron will be at the Contractor's option, unless otherwise shown on project plans.



STATE OF CALIFORNIA  
DEPARTMENT OF TRANSPORTATION

STANDARD PLANS UNIT, 1205 L STREET, SACRAMENTO, CALIFORNIA 95833

REVISED DUAL UNITS STANDARD PLAN RSP A88A

ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE SHOWN

RSP A88A DATED 9-29-04 SUPERSEDES RSP A88A DATED 03-12-04 AND RSP A88A DATED JULY 1, 2002-PAGE 03 OF THE STANDARD PLANS BOOK DATED JULY 2002.

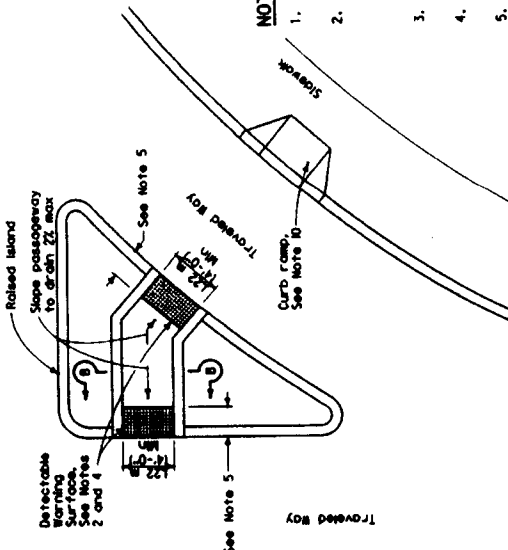
NO SCALE

These Standard Plans for Construction of Local Streets, Intersecting Curbside Units in Two Systems of Intersecting Streets are shown in the parentheses (1). The units are not necessarily used in the two systems or in the same order as indicated in the parentheses. See the Foreword of this publication.

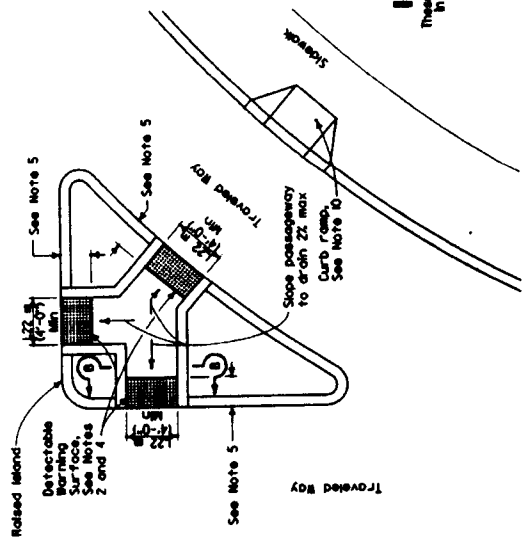
83

DIST.	COUNTY	ROUTE	PROJECT	DATE	TOTAL SHEETS

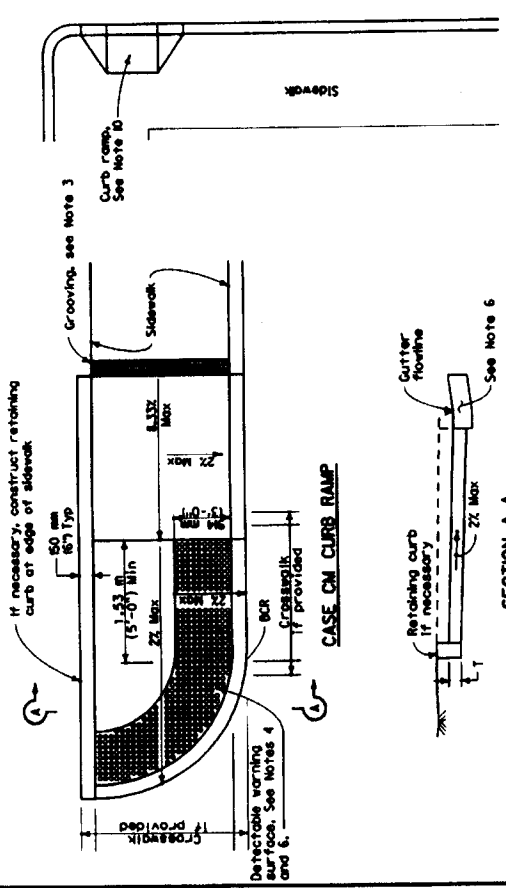
**REGISTERED CIVIL ENGINEER**  
 H. Paul Cook  
 No. 45182  
 State of California  
 9-29-04  
 The State of California, by the Office of the State Architect, has approved the construction of this project for the purpose of providing a safe and sound structure for the use of the public.



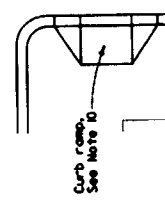
**TYPE B PASSAGEWAY**



**TYPE C PASSAGEWAY**



**TYPE A PASSAGEWAY**



**NOTES**

1. Sidewalk, ramp and passageway thickness, "T", shall be 90 mm (3 1/2") minimum.
2. Minimum width of passageway through raised islands shall be 2.44 m (8'-0") clear for locations where right of way restrictions, natural barriers, or other existing conditions create an unreasonable hardship, the clear width of the passageway may be reduced to 915 mm (3'-0").
3. For details of grooving used with Case CM curb ramp, see Revised Standard Plan RSP 800B.
4. For details of detectable warning surfaces, see Revised Standard Plan RSP 800B.
5. Where an island passage way length is less than 1.8 m (6'-0"), the detectable warning surface shall extend the full width and full depth of the island. Where an island passage way length is greater than or equal to 1.8 m (6'-0") but less than 2.44 m (8'-0"), a detectable warning surface shall extend the full width and 600 mm (2'-0") depth of the passage way length. Where an island passage way length is greater than or equal to 2.44 m (8'-0"), a detectable warning surface shall extend the full width and 1.14 m (3'-0") depth of the passage way length.
6. For Case CM curb ramp, the edge of the detectable warning surface nearest the street shall be between 150 mm (6") and 205 mm (8") from the gutter line.
7. Transitions from ramps to walks, gutters or streets shall be flush and free of abrupt changes.
8. Maximum slopes of adjoining gutters, the road surface immediately adjacent to the curb ramp and continuous passage to the curb ramp shall not exceed 5 percent within 1.22 m (4'-0") of the top or bottom of the curb ramp.
9. Utility pull boxes, manholes, vaults and all other utility facilities shall be flush with the curb ramp. The curb ramp will be adjusted to grade by the owner prior to, or in conjunction with, curb ramp construction.
10. For additional curb ramp details, see Revised Standard Plan RSP 800B.

STATE OF CALIFORNIA  
 DEPARTMENT OF TRANSPORTATION

### ISLAND PASSAGEWAY AND CURB RAMP DETAILS

These Standard Plans for Construction of Lead Streets and Roadway contain units in two systems of measurement: International System of Units (SI) and United States Standard Customary System (USCS). The units in each system are shown in parallel columns. The units in each system are not necessarily equal or interchangeable. See the "Foreword" of this publication.

NO SCALE

ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE SHOWN

REVISED DUAL UNIT STANDARD PLAN RSP 800B